



# NEBRASKA CAPITOL ENVIRONS COMMISSION

The Nebraska Capitol Environs Commission will hold a meeting on **Friday, July 28, 2023 at 8:30 a.m.** in the County-City Building, 555 S. 10th Street, Lincoln, Nebraska, in **City Council Chambers** on the 1<sup>st</sup> Floor. For more information, please contact the Lincoln City/Lancaster County Planning Department at 402-441-7491.

---

## AGENDA

July 28, 2023

1. Approval of meeting record from [June 23, 2023](#)

### Public Hearing & Action

2. Phase I modifications at [220 Centennial Mall S](#) (*White Lotus, UDR23093*)
3. Landscape improvements at [1821/1843 K Street](#) (*UDR23094*)
4. Bike racks in front of [1023 Lincoln Mall](#) (*UDR23091*)

### Discuss & Advise

5. Staff report & miscellaneous

**ACCOMMODATION NOTICE:** *The City of Lincoln complies with Title VI of the Civil Rights Act of 1964 and Section 504 of the Rehabilitation Act of 1973 guidelines. Ensuring the public's access to and participating in public meetings is a priority for the City of Lincoln. In the event you are in need of a reasonable accommodation in order to attend or participate in a public meeting conducted by the City of Lincoln, please contact the Lincoln Commission on Human Rights at 402-441-7624, or the City Ombudsman at 402-441-7511, as soon as possible before the scheduled meeting date in order to make your request.*

## MEETING RECORD

*Advanced public notice of the Nebraska Capitol Environs Commission meeting was posted on the County-City bulletin board and the Planning Department's website. In addition, a public notice was emailed to the Lincoln Journal Star for publication on Wednesday, June 15, 2023.*

**NAME OF GROUP:** NEBRASKA CAPITOL ENVIRONS COMMISSION

**DATE, TIME AND PLACE OF MEETING:** Friday, June 23, 2023, 8:30 a.m., City Council Chambers, County-City Building, 555 S. 10<sup>th</sup> Street, Lincoln, Nebraska.

**MEMBERS IN ATTENDANCE:** Andrea Gebhart, Delonte Johnson, Kile Johnson, Karen Nalow and Ann Post; (Heidi Cuca and David Quade absent).

**OTHERS IN ATTENDANCE:** Collin Christopher and Teresa McKinstry of the Planning Department; John Hathaway with Architectural Design Associates; Matt Hansen with the Nebraska Capitol Commission; Bob Ripley; and other interested citizens.

**STATED PURPOSE OF MEETING:** Nebraska Capitol Environs Commission Meeting

Chair K. Johnson called the meeting to order and acknowledged the posting of the Open Meetings Act in the room.

K. Johnson then called for a motion approving the minutes of the regular meeting held April 28, 2023. Motion for approval made by Nalow, seconded by Gebhart and carried 5-0: Gebhart, D. Johnson, K. Johnson, Nalow and Post voting 'yes'; Cuca and Quade absent.

### **NEW CONSTRUCTION AT 440 S. 13<sup>TH</sup> STREET**

#### **PUBLIC HEARING:**

**June 23, 2023**

Members present: Gebhart, D. Johnson, K. Johnson, Nalow and Post; Cuca and Quade absent.

Collin Christopher stated that this project was reviewed in April. The applicant was directed to come back with a final landscape plan and to explore options for screening the rooftop equipment. He summarized that on a site like this in downtown, there are two requirements – screening around the parking lot and providing street trees where room allows. In theory, for a downtown parking lot, that means that all sides need to be screened with landscaping. He stated that there was some concern from the applicant about screening along the north and east sides. The north side is abutting an alley without a curb. There was general concern about being able to maintain anything that is planted there. The east side runs up against a building, and perhaps it doesn't make a lot of sense to place landscaping there either. As is often done, staff worked with the applicant to come up with a solution that meets the intent of the design standards.

There was a compromise discussed to not have landscaping on the north and east sides of the parking lot, but to have some additional landscaping elsewhere that would not normally be required. The foundation plantings shown around the building are extra landscaping the applicant is choosing to provide. Additionally, in the right-of-way, the landscape beds along 13<sup>th</sup> Street will be restored and the area that used to be an access drive along that side will be filled in with additional landscaping. The beds at the southeast corner of the site will be restored as well. He believes this is a reasonable compromise. There are two smaller trees on the west side. In a perfect world we would like them to be larger, but there are power lines there today. If the existing trees were to be removed, it would be unlikely that LES would allow them to be replaced. Finally, a tree could be located at the southwest corner, but LTU would likely not support a tree so close to the existing traffic signal. He pointed out a location on 'K' Street where two new trees would be planted just outside of the right-of-way. Staff is supportive of the landscape plan that was submitted.

John Hathaway stated that regarding the rooftop screening, they looked at two possible options. One was to provide a full screening wall around all assemblies. The price was \$150,000.00. They also looked at individual clip on screens that were around \$15,000.00 per unit and there are seven units. The budget on this project is limited. The owner was resistant to implementing either of those. Construction costs have gone up. They did a site line study from the ground level. It is his understanding that the concern was visibility from the walkway. A pedestrian or driver would need to be 430' away from the building before the rooftop units become visible. He is hoping this gives the Commission enough confidence to allow the project to move forward.

Gebhart questioned the landscaping. She inquired if Lincoln Transportation and Utilities (LTU) is on board with restoring the beds and connecting them. Christopher responded they are the ones that are dictating the drive be removed. LTU doesn't get very involved with landscaping unless it is taller than 30" or 36" in height.

Post requested an explanation about the planting compromises that were made. How is it determined that certain requirements can be waived by offsetting them with additional landscaping in other areas? Christopher noted that staff was trying to provide an option that benefitted the district. One of the restrictions on this site is the applicant has chosen to build in the old footprint. They haven't changed any of the layouts. In a different design scenario, they might have had more flexibility to strictly meet the standards. The applicant has very little room on the north side to plant anything. There also some maintenance concerns on the east side. From his perspective, staff tries to look for ways in which the intent of the standards can be met. He thinks on this one, as with a lot of downtown properties, he believes the goal is to find areas where landscaping can make the most impact. Not doing landscaping on an alley, but in the right-of-way seems like a fair tradeoff. This isn't a rule. Staff was just looking for the most benefit. Post appreciates that there are practical realities.

K. Johnson wondered if there are design standards for rooftop screening we should be concerned with. Christopher stated the only rooftop screening standards address those properties directly facing Capitol Square. Otherwise, there aren't any specific standards that dictate rooftop equipment needs to be screened. K. Johnson looked at the property. The parking on the east side goes right up to the building. He

can see the impracticality of screening in the alley. He finds it unfortunate they do not want to screen the rooftop equipment.

D. Johnson indicated his appreciation for all the background information that staff provides.

Nalow appreciates the alternatives provided. We all benefit from increasing the green. Knowing there is new construction and existing trees that are being saved, she would recommend making sure there is adequate language in the construction documents to protect the trees. She would urge the applicant to take a little more time and think about the species that is selected. White basswood in an urban condition is sometimes a little more sensitive. It doesn't always thrive well in urban conditions. She would also urge the applicant to think about the balance of the perennials and shrubs in the right-of-way and around the building. She has observed how some perennial species thrive and some have a hard time in the urban environment with salt and snow. She knows there are limitations with LTU and height restrictions less than 30 inches. Hathaway will share that information with the landscape architect.

**ACTION:**

Post moved approval of a Certificate of Appropriateness for the landscape plan as presented, seconded by D. Johnson and carried 5-0: Gebhart, D. Johnson, K. Johnson, Nalow and Post voting 'yes'; Cuca and Quade absent.

**BIKE RACKS AT 1023 LINCOLN MALL:**

Christopher stated this came in late in the process. Staff was unable to advertise this item for the agenda. It could perhaps come back for a public hearing next month. The Planning Dept., in coordination with LTU and other departments through the Complete Streets Committee, is implementing a bike rack request program. Businesses can request a bike rack in front of their business to be paid for and installed through the Complete Streets fund. They received a request from Lincoln Literacy that they would like two bike racks. The committee's exploration of that request looked at placing those behind the building. LTU had concerns about how that would impact ADA access to the building. The committee's second preferred location would be to have the racks directly off the mall. He pointed out the location in front of the door at 1023 Lincoln Mall. There would be two racks that each hold two bikes. They would likely have a silver powder coat finish and be surface mounted into the concrete. They are looking for advice and direction if this is an appropriate solution, or if staff should continue to look at alternate locations.

K. Johnson noted that this issue came up for a Certificate of No Material Effect. There are parking stalls behind the building. He personally has concerns with it being on the mall. He believes it is inappropriate to have the bike parking structure in front of the building.

Nalow noted that was her initial concern as well, along with thinking of the precedent that could be set. She would be concerned that any future renovation would allow more of these.



Gebhart questioned what door is the main entrance. Christopher believes people driving cars are obviously entering from the back. The request was made because there are at least 3-4 people who regularly ride their bikes to Lincoln Literacy on a regular basis.

Bob Ripley believed there were some general standards developed by the improvement project that was done by the Nebco building. He asked about their location of bike racks. Nalow stated they are in the back of the building. Ripley recalls that was a TIF (Tax Increment Financing) project larger than the site. He believes that once the TIF project was approved, there was a whole system of benches and bus stops, which included all of Lincoln Mall. He would think once that was approved, that would be the standard of Lincoln Mall in terms of location of bike racks. He believes that parking is parking. His assumption is that parking ought to be in the back of the building. He believes the comment about these racks occurring in front of the buildings is relevant. If there is an alternative, he would think putting the racks where the remainder of the parking is would be appropriate.

Nalow wasn't involved in the Lincoln Mall design, but at that time she believes that bike racks weren't involved in the plan. Bike racks were in the back of the building. She believes the same developer has provided bike parking in either the back or within the garage space that is part of those buildings. Ripley agreed. They were not a feature in the public right-of-way.

K. Johnson doesn't believe there are currently any bike racks along Lincoln Mall. Some people attach their bikes around the bus stops. That is less than desirable. Ripley stated there are some bike racks in parking lots, but it is not a feature that you typically see from the primary face of the mall.

K. Johnson inquired if there are any standards that cover bike racks along the mall. He noticed standards say mechanical equipment shall not be located in the front yard. Christopher would classify this as a site furnishing. He believes there are some references to furnishings, but not bike racks specifically. The reality is that this isn't designed as part of a larger streetscape. He knows they have tried to look at other locations. He doesn't know if doing it in a parking stall would be something they would consider, or if it would be allowed. Staff could ask them to explore that as an alternative.

Nalow has seen that approach work in the right-of-way. By Cultiva, there were bike racks put in place of a parking stall. She believes there are two ways to look at this. It is a site furnishing, but also parking.

Post can see both sides of the issue. It would be an amenity to a lot of people and she understands wanting to have them in a high visibility area. She also understands not wanting to see bikes parked outside of a building. To her knowledge, the redevelopment plan that was put in place for this area wouldn't have any purview for this Commission. She would like that clarified if this is something that should be looked at. She would think that this is something Urban Design Committee reviewed. Christopher believes Post is correct. He doesn't believe that any previous action on redevelopment projects binds us to future actions. Post believes that Nebraska Capitol Environs Commission needs to look at codes, but Urban Design Committee has broader powers to give recommendations. Christopher noted this is a City led project. He doesn't believe they would go against this body's recommendation.

Gebhart is torn. The downtown area has a lot of bike traffic and if we want to accommodate that traffic, the bike rack is one option. If the racks are tucked behind the building, they become more of a business-specific benefit. It feels like a broader mall benefit to have them located along the mall where they can be used by others, but she is concerned about the long-term maintenance of the racks. Are these racks going to be properly maintained over the years? This is a concern given how much of a visual asset Lincoln Mall is for the community, but she is torn because she wants to make Lincoln Mall more of a community space.

Nalow stated if we are looking at this being specifically provided for this property owner or as a district thing, perhaps it could be along another right-of-way and not on the mall. Perhaps it could be located on 11<sup>th</sup> Street or 12<sup>th</sup> Street. If this is not an amenity for the larger area, but this specific property, she believes there is concern for the precedent that it sets. If it is more for a private entity, it makes more sense to have it in an area they control. Gebhart agreed with Nalow. Is it a community asset or a private asset? Post would agree as well. It would be in the best interest of the property owner to move it to a less visible location.

Christopher will take the comments to the committee.

**MISCELLANEOUS:**

- K. Johnson inquired if anyone had any information regarding 1515 'F' Street. The windows appear to be 95 percent done, but nothing else has happened. Christopher knows it has changed ownership a few times, which is why work appears to have started and stopped a few times. He remembers seeing some additional permitting come forward recently. He will review and report back.

There being no further business, the meeting was adjourned at 9:10 a.m.

<https://linclanc.sharepoint.com/sites/PlanningDept-Boards/Shared Documents/Boards/NCEC/Minutes/2023/062323.docx>

## NEBRASKA CAPITOL ENVIRONS COMMISSION STAFF REPORT

APPLICATION NUMBER	Urban Design Record #22093
APPLICATION TYPE	Certificate of Appropriateness
ADDRESS/LOCATION	220 Centennial Mall S
HEARING DATE	July 28, 2023
ADDITIONAL MEETINGS	February 25, 2022 (original hearing date)
APPLICANT	Drew Sova, 402-408-0005, dsova@whitelotusgroup.com
STAFF CONTACT	Collin Christopher, 402-441-6370, cchristopher@lincoln.ne.gov

**RECOMMENDATION: N/A**

### Summary of Request

In February of 2022, NCEC reviewed and approved a Certificate of Appropriateness for Phase I of the Pershing block redevelopment project. The proposal included the construction of a new building consisting of approximately 90 affordable housing units and some ground floor commercial fronting S 16<sup>th</sup> Street. As the redevelopment process has progressed in the intervening seventeen months, the developer has furthered their building and site plans, including some modifications to the plans reviewed by this Commission.

While minor modifications are commonplace in the design development process, staff is of the opinion that there are enough changes to the previously approved plans to warrant another review by NCEC. Those changes can be separated out into two categories:

- **Façade modifications** – The most noticeable façade change relates to the color palette for the façade. The original plans submitted in 2022 showed a light brick product on the lower level with a darker brick on the upper levels. The updated plans offer a reverse approach – light brick below and dark brick above. Beyond that, the widow placement/transparency appears to have been modified in subtle ways that do not appear to have a material effect on the quality of the facades.
- **Building setback modifications** – The 2022 plans were processed and reviewed by staff with the assumption that the building was being built to the M, N and 16<sup>th</sup> Street property lines, as is standard for new development in downtown Lincoln. This was not a topic of discussion at that February 2022 meeting, per the approved minutes. The modified proposal shows a 3’ setback from S 16<sup>th</sup> Street, and a 4.5’ setback on the M and N Street sides.

In discussions with the applicant regarding the building setbacks, staff has conveyed a desire to have the project be built to the property lines, per the Downtown Design Standards. However, the applicant has made a case for why the small setbacks are necessary. Per the application material submitted, those reasons are as follows:

<https://linclanc.sharepoint.com/sites/PlanningDept-Boards/Shared Documents/Boards/NCEC/REPORTS/2023/07-July/Pershing Block Phase I staff report.docx>

1. Affordable housing unit layout design is driving building footprint size. There is not sufficient additional space in the north/south parcel lot dimension for an additional housing unit to be integrated in the plan layout.
2. Design space is needed at the north along N street to locate intake air for underground parking ventilation, and also at the south along M street for exhaust air. This chase and its supporting structure must be included on the project property extents.
3. Indicated offsets from the front property lines allow that building footings will be able to be designed and constructed within project property boundaries, and will avoid any potential conflicts with City of Lincoln public easements for utilities.

Given the location of this project within the Capitol Environs District, NCEC has the full authority to determine whether this justification is reasonable or not.

Per the direction of city staff, the streetscape plans have not been submitted at this time. Staff is still in ongoing conversations with the applicant to finalize a preferred streetscape approach. Once that approach has been agreed to, a separate certificate will need to be considered by this Commission. However, an updated Certificate for the building- and site-related work in front of the Commission will allow the applicant to move forward with building permits for that portion of the project.

### **Compatibility with the Design Standards**

Analysis of design standards compatibility should rely most heavily on the Capitol Environs Design Standards (Chapter 3.85), but should also consider any relevant elements of the Downtown Design Standards (Chapter 376). What follows is a collection of the most relevant standards from each.

#### **Chapter 3.85, Design Standard 9: Facades**

---

New buildings in the District should be designed to enhance the setting of the Capitol and their immediate surroundings. When those surroundings have a high degree of cohesiveness, new designs should be compatible with their setting, strengthening the visual relationships found among existing buildings and landscape features. In areas that lack cohesion, designs should be proposed that offer themes and patterns that can be further expanded in future development.

Brick, stone, or other richly textured, highly durable masonry is desirable for building exteriors on Capitol Square, Centennial Mall, and Lincoln Mall. Permanence should be an overriding characteristic in the choice of exterior materials. Colors should be drawn from a muted palette of warm, earth tones or shades of white, with the context of surrounding buildings as a guide.

In the rehabilitation of existing buildings, retention of high-quality materials and use of new, durable, and high-quality materials is also desirable.

#### **Chapter 3.85, Guideline 9.1**

Proposals for new buildings should strengthen interrelationships among buildings within a specific setting, while encouraging variations. Features that contribute to compatibility among buildings include similarities in:

- alignment and setback;
- spaces between buildings;
- silhouette, including height and roof pitch;
- building base--relationship of building to ground or site;
- materials and material scale;

<https://linclanc.sharepoint.com/sites/PlanningDept-Boards/Shared Documents/Boards/NCEC/REPORTS/2023/07-July/Pershing Block Phase I staff report.docx>

- mass and scale;
- building shade and shadow pattern from massing;
- permanence and durability, with 100-year "life cycle" expected facing Capitol Square and 75 year "life cycle" expected on Centennial and Lincoln Malls;
- entrance position, scale, and features;
- color, finish, and texture;
- size, type, and proportion of openings;
- ornamentation and detail, particularly at street level and in the residential areas;
- landscape design and features;
- cornice heights.

#### Chapter 3.85, Guideline 9.5

Metal is not a suitable primary material for building exteriors in the District.

#### Chapter 3.85, Guideline 9.6

Non-concrete stucco-like materials are discouraged from use on Capitol Square or Centennial and Lincoln Malls, especially on ground floors.

#### Chapter 3.85, Guideline 9.7

Wood is not a suitable primary material for building exteriors on Capitol Square or Centennial and Lincoln Malls.

#### Chapter 3.85, Guideline 9.10

On Centennial and Lincoln Malls, balconies, terraces, and other indoor/outdoor elements should be set back from the main plane/built-to line of the mall facade.

#### Chapter 3.76, 4.1 Site Development

a. Downtown Lincoln buildings west of 19<sup>th</sup> Street and on North 21<sup>st</sup> Street from O to Q Streets shall be substantially "built-to" their front property lines (and on corner lots, shall be built-to both front property lines).

**Compatibility per Staff Analysis:** The updated plans largely meet the intent of both sets of design standards. The exception to that would be the setback issue previously summarized. The Downtown Design Standards clearly suggest that new buildings be built to the property line, while the Capitol Environs Design Standard focus more on consistent setbacks amongst adjacent properties. Looking at this block and those that surround it, that consistency does not really exist today. Many buildings are built to the property, but there are also a number that are not. Given these two slightly different standards, staff is of the opinion that this is an example where the Downtown Design Standards should typically prevail, allowing that consistent approach of no setback to occur over time as sites get redeveloped.

#### Recommendations

In comparing the plans approved in February 2022 with those submitted as part of this review, staff believes that the façade-related changes do not result in a diminished project. The changes in the masonry choices are a matter of preference and not of quality or durability. Likewise, the modifications to the

<https://inlanc.sharepoint.com/sites/PlanningDept-Boards/Shared Documents/Boards/NCEC/REPORTS/2023/07-July/Pershing Block Phase I staff report.docx>

windows/transparency appear to simply be a matter of accommodating updated floor plans and anticipated tenant needs.

Staff's perspective regarding the setbacks is a little more nuanced. The Downtown Design Standards are clear in their expectations and allowing a deviation of this nature impacts not only future urban design decisions on this block but also on the blocks that surround it. At the same time, one might argue that a setback of 3' or 4'-6" is not a distance that most will notice, and that the value add of the affordable housing and other uses to be contained within this block are much more important and relevant to the future of downtown and the Capitol Environs District. Ultimately, staff understands the complex nature of a project like this and seeks NCEC's guidance on how to proceed.



ATTACHMENT A – Location Map



<https://linclanc.sharepoint.com/sites/PlanningDept-Boards/Shared Documents/Boards/NCEC/REPORTS/2023/07-July/Pershing Block Phase I staff report.docx>

## ATTACHMENT B – Renderings



*The images on the left represent the design approved in February 2022 and the images on the right show the modified design submitted for an updated certificate.*

<https://linclanc.sharepoint.com/sites/PlanningDept-Boards/Shared Documents/Boards/NCEC/REPORTS/2023/07-July/Pershing Block Phase I staff report.docx>



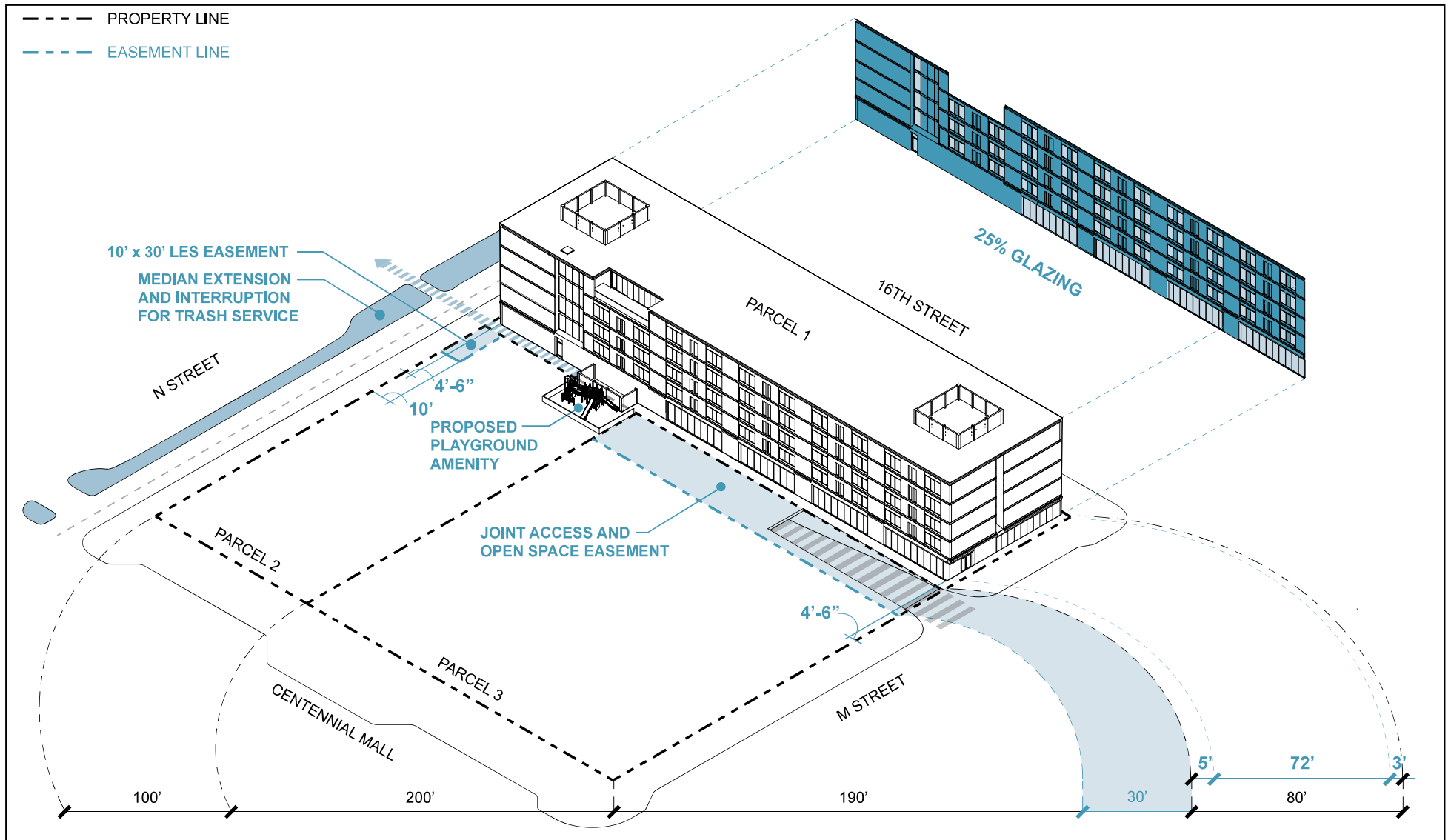


# MURAL HOUSING PROJECT

NEBRASKA CAPITOL ENVIRONS COMMISSION  
PRE-CONSTRUCTION PROJECT REVIEW

07.28.2023





Note: Streetscape improvement design to be developed and reviewed at a later date.

### SITE DIAGRAM

## CITY OF LINCOLN DOWNTOWN DESIGN STANDARDS

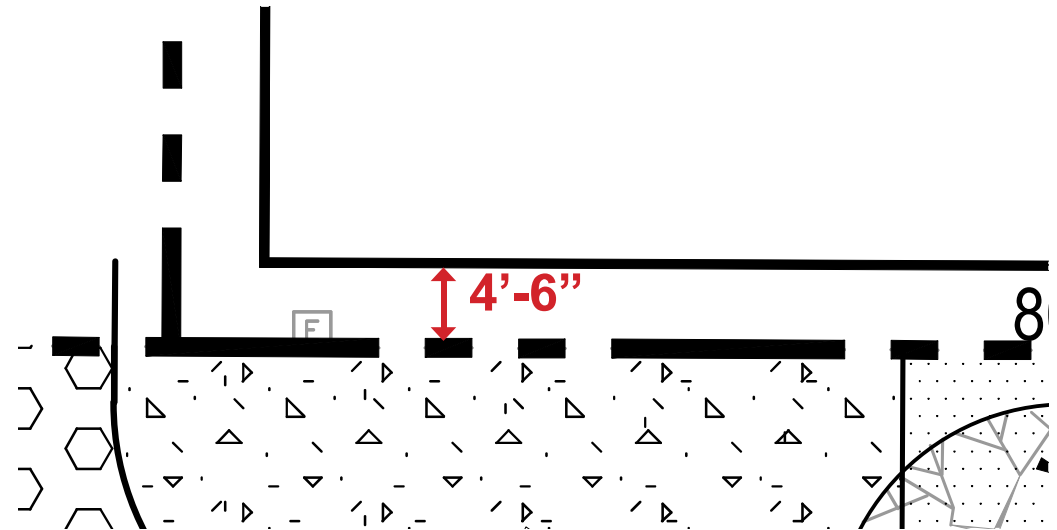
### Section 4. GENERAL REQUIREMENTS

#### 4.1 Site Development

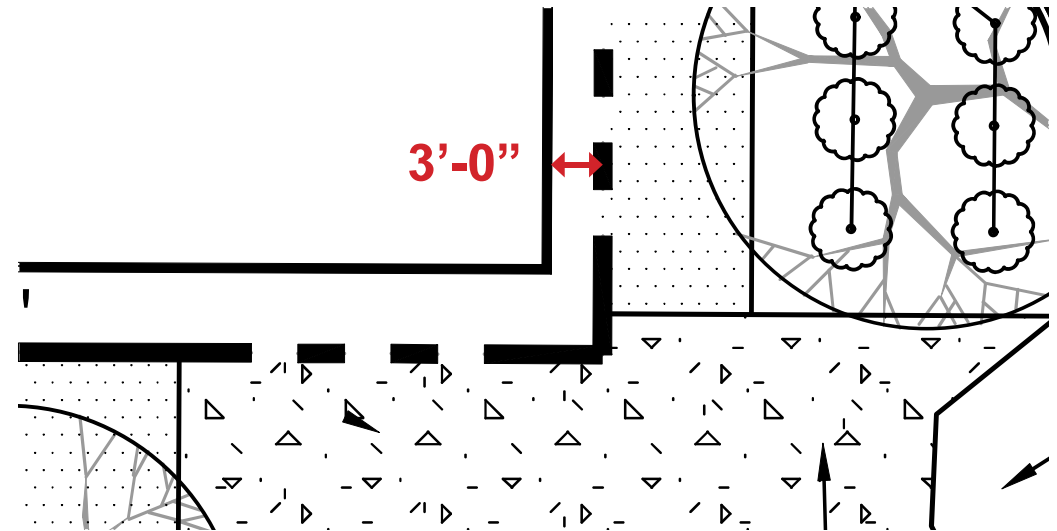
*Downtown Lincoln buildings west of 19th Street and on North 21st Street from O to Q Streets shall be substantially "built-to" their front property lines (and on corner lots, shall be built-to both front property lines).*

### DESIGN DRIVERS FOR PROPOSED DESIGN SOLUTION

1. Affordable housing unit layout design is driving building footprint size. There is not sufficient additional space in the north | south parcel lot dimension for an additional housing unit to be integrated in the plan layout.
2. Design space is needed at the north along N street to locate intake air for underground parking ventilation, and also at the south along M street for exhaust air. This chase and its supporting structure must be included on the project property extents.
3. Indicated offsets from the front property lines allow that building footings will be able to be designed and constructed within project property boundaries, and will avoid any potential conflicts with City of Lincoln public easements for utilities.



### M AND N STREET SETBACK CONDITIONS



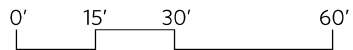
### 16TH STREET SETBACK CONDITION



**NORTH ELEVATION**

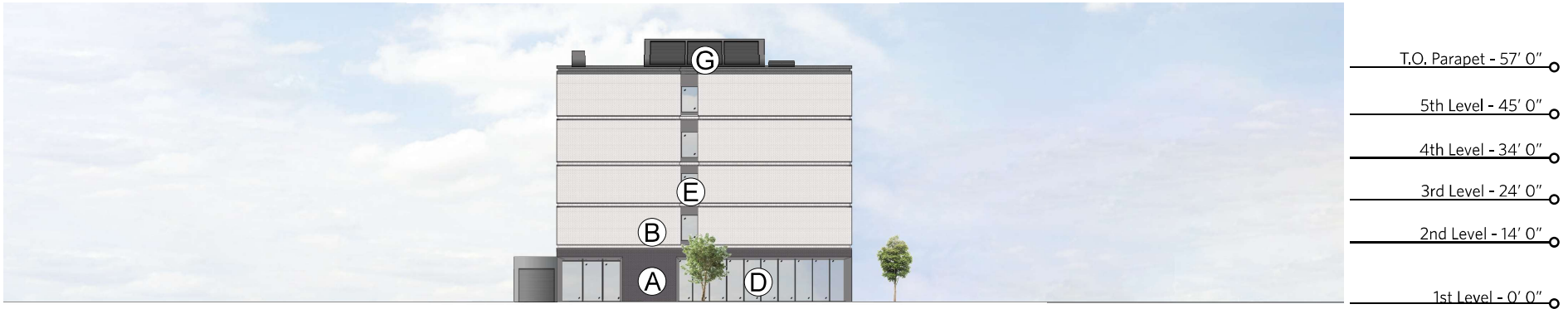


**EAST ELEVATION**



**Material Legend**

- A. Medium Brick Podium
- B. Light Brick Facade
- C. Cementitious Panel System Accent
- D. Aluminum Framed Storefront System
- E. Fixed Windows
- F. Ventilation Louvers
- G. Mechanical Screening



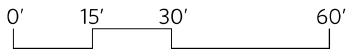
**SOUTH ELEVATION**

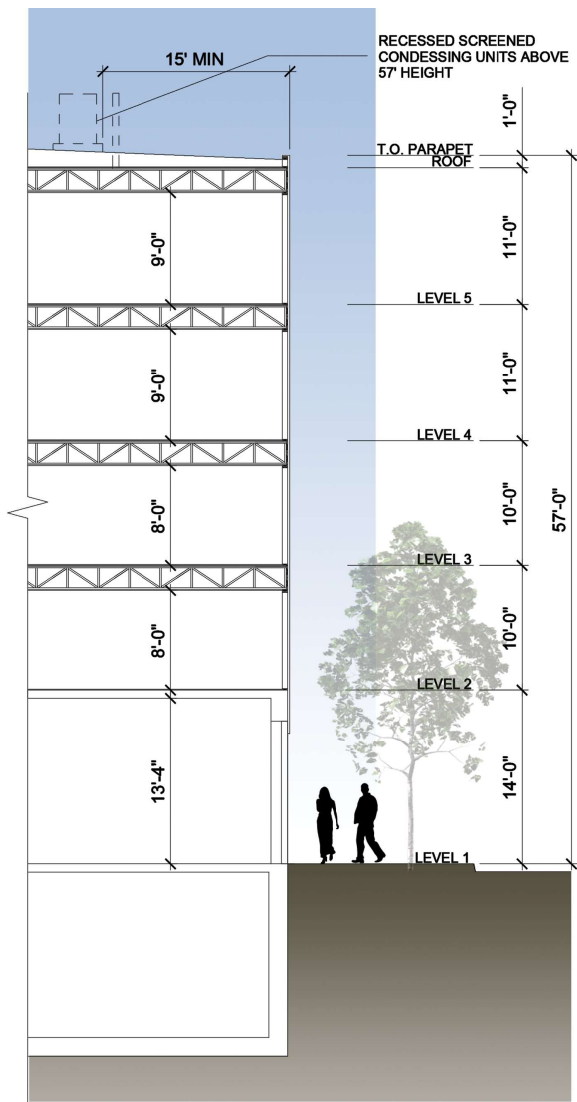


**WEST ELEVATION**

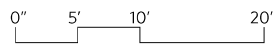
**Material Legend**

- A. Medium Brick Podium
- B. Light Brick Facade
- C. Cementitious Panel System Accent
- D. Aluminum Framed Storefront System
- E. Fixed Windows
- F. Ventilation Louvers
- G. Mechanical Screening





**SECTION - BUILDING OVERALL HEIGHT**





## **Primary Material Basis of Design**

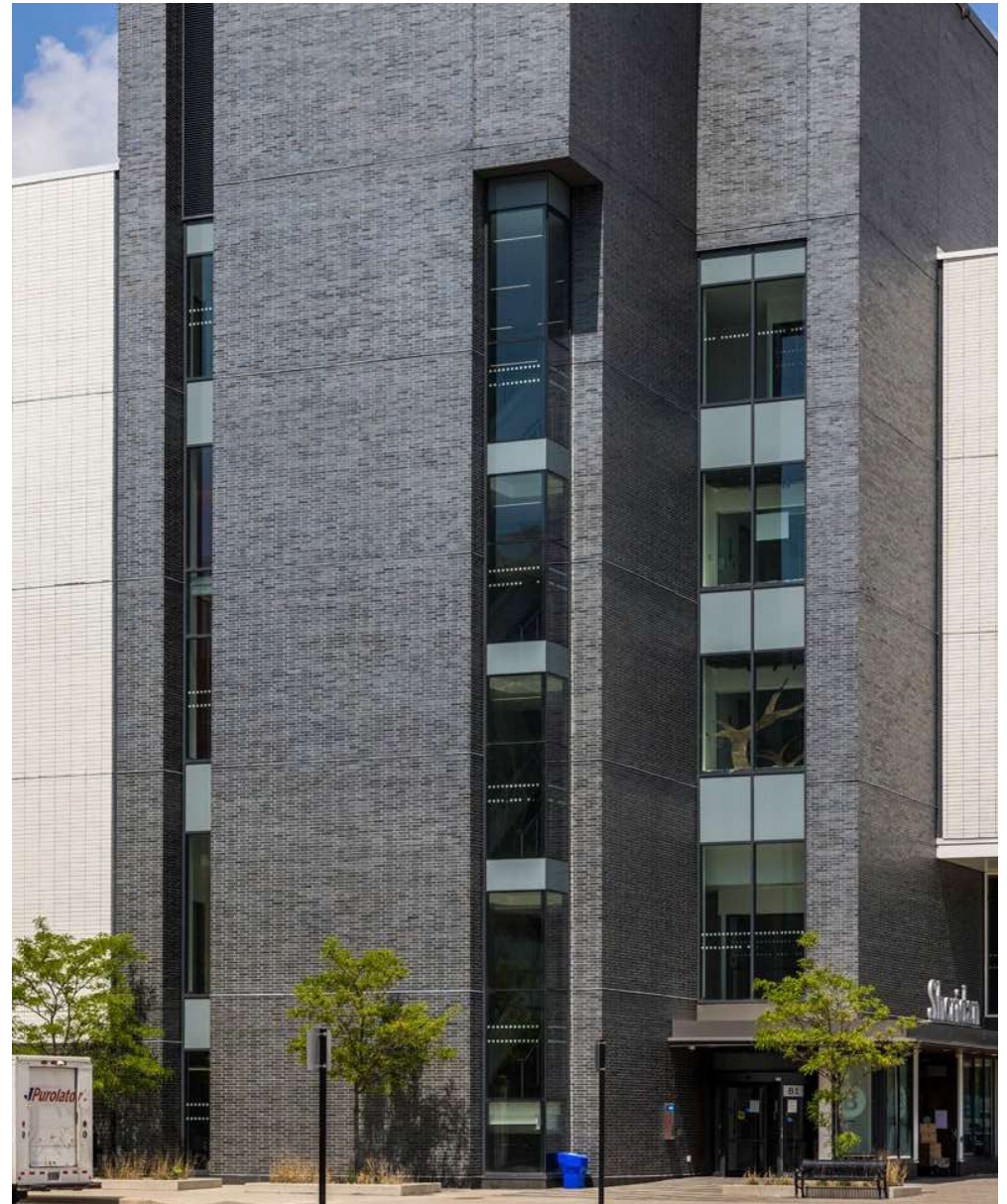
### A. Medium Gray Brick Podium

Description: Modular face brick over a steel framed podium.  
The darker/cooler tone at the base will compliment the lighter/warmer tones of the brick above.

Location: Ground Level exterior facade material.

Color: Ebonite by Glen-Gery

Texture/Finish: Smooth



## **Primary Material Basis of Design**

### B. White Face Brick

Description: Modular face brick over exterior wood framing. A lighter/warm toned brick will work with the limestone textures within the Capitol Environs District and compliment the cooler/darker tones of the base.

Location: Housing Levels of Project (2-5).

Color: Aspen White by Glen-Gery

Texture/Finish: Smooth





## **Accent Material Basis of Design**

### C. Fiber Reinforced Cementitious Panel

Description: Durable, factory-finished panelized rain-screen facade cladding for the residential portions of the project. System to be designed with warm tones referencing a wood finish. Also includes a medium gray finish accent material located at the head and sill of window openings.

Location: Housing Levels of Project (2-5), Facade Recess Locations

Color: Clay, Pewter

BOD: Novenary Dimension Series by Nichiha Fiber Cement



## **Glazing Material Basis of Design**

### **D. Aluminum Framed Storefront**

Description: 70% PVDF finished aluminum storefront framing systems at the ground level mixed-use and retail zones and vertically glazed sections (Storefront system structurally broken at each floor slab).

Location: Ground Level and Vertical Glazing Areas

Color: Black/Dark Bronze Frames

BOD: Kawneer 451t Series Framing



### **E. Fixed Windows**

Description: Durable, prefinished windows located at the housing portion of the project.

Location: Housing Levels of Project (2-5)

Color: Factory-Finish, Black

BOD: Pella Encompass Series Windows



### **F. Aluminum Louvers**

Description: Aluminum exterior louvers designed for residential VTAC units. Factory-finished products will be integrated into the facade to match adjacent materials.

Location: Housing Levels of Project (2-5)

Color: Match Material B.

BOD: Airolite K630 Drainable Louver







**VIEW NORTHEAST CORNER**



**VIEW NORTHWEST CORNER**





**VIEW SOUTHWEST CORNER**





**VIEW SOUTHEAST CORNER**





**CE Presentation - 02.25.2022**

**Current Design - 07.18.2023**





**Material Precedents**

- A. Sky Park Apartments
- B. St. Mary's Catholic Church
- C. Parkhaus Apartments
- D. Bennett Martin Public Library
- E. Pershing Auditorium

**CAPITOL ENVIRONS MATERIAL USE PRECEDENTS**



## NEBRASKA CAPITOL ENVIRONS COMMISSION STAFF REPORT

APPLICATION NUMBER	Urban Design Record #22094
APPLICATION TYPE	Certificate of Appropriateness
ADDRESS/LOCATION	1821/1843 K Street
HEARING DATE	July 28, 2023
ADDITIONAL MEETINGS	July 22, 2022 and December 23, 2022
APPLICANT	Greg Newport, 402-430-5631, <a href="mailto:greg.newport@outlook.com">greg.newport@outlook.com</a>
STAFF CONTACT	Collin Christopher, 402-441-6370, <a href="mailto:cchristopher@lincoln.ne.gov">cchristopher@lincoln.ne.gov</a>

### RECOMMENDATION: APPROVAL

#### Summary of Request

In July of 2022, Community Action applied for and received a Certificate of Appropriateness to allow them to add vehicle barriers north of their building at 1821/1843 K Street. Per their original application letter, the barriers are intended to prevent vehicles failing to make the turn eastward off of Antelope Valley Parkway from hitting the building. The original plans showed ten (10) spherical traffic barriers made of solid concrete. They were to have a diameter of 48” with a spacing of 48”. The spherical barriers were to be “painted to simulate large marbles.”

In December of 2022, the applicant submitted a revised design for the barrier system. The new plans eliminated the spherical barriers and replaced them with a cable restraint system that they planned to landscape to offset any negative visual impacts. Again, they were granted a Certificate of Appropriateness under the condition that they provide the Commission a landscape plan for review prior to planting.

They have now submitted such a plan and are looking for approval from the Commission to proceed with installation of that plan.

#### Compatibility with the Design Standards

Like the barrier itself, the landscaping will front the K Street side and does not have a direct visual impact on the J Street landscape that is of most concern in the Capitol Environs District. As such, the Design Standards offer limited guidance beyond an expectation that the landscaping be designed with maintenance and replacement in mind.

#### Design Standard 27: Landscape Maintenance and Replacement

Landscape improvements, especially those on public property, should be designed and installed for economical maintenance and programmed replacement as needed.

## Recommendations

While the proposed landscape plan appears to meet the intent of the design standards, the Commission should weigh in on any specific concerns related to plant species, spacing, or maintenance of the proposed landscape improvements.

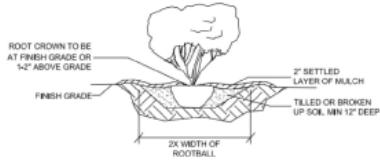
**Recommended finding:** The proposed landscape plan complies with the Capitol Environs Design Standards.

**Recommended action:** Approval of an updated Certificate of Appropriateness for the proposed work at 1821/1843 K Street.

ATTACHMENT A – Location Map



# ATTACHMENT B – Landscape Plan



**1** **Planting Detail**  
 SCALE: Not to Scale

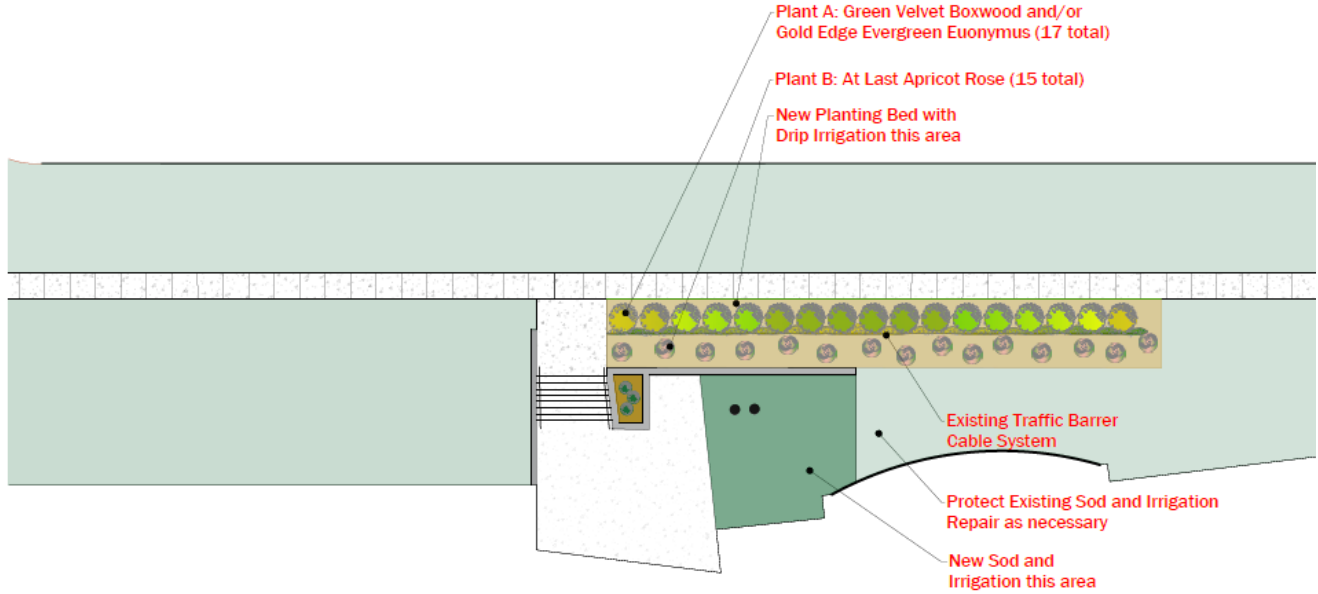


**1** **Green Velvet Boxwood**  
 SCALE: Not to Scale



**1** **At Last Apricot Rose**  
 SCALE: Not to Scale

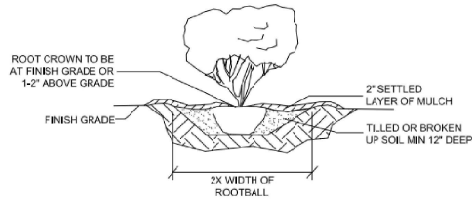
See Spec 4 for Information on Plant Species





ATTACHMENT C – Traffic Barrier Example Image





**1** Planting Detail  
SCALE: Not to Scale

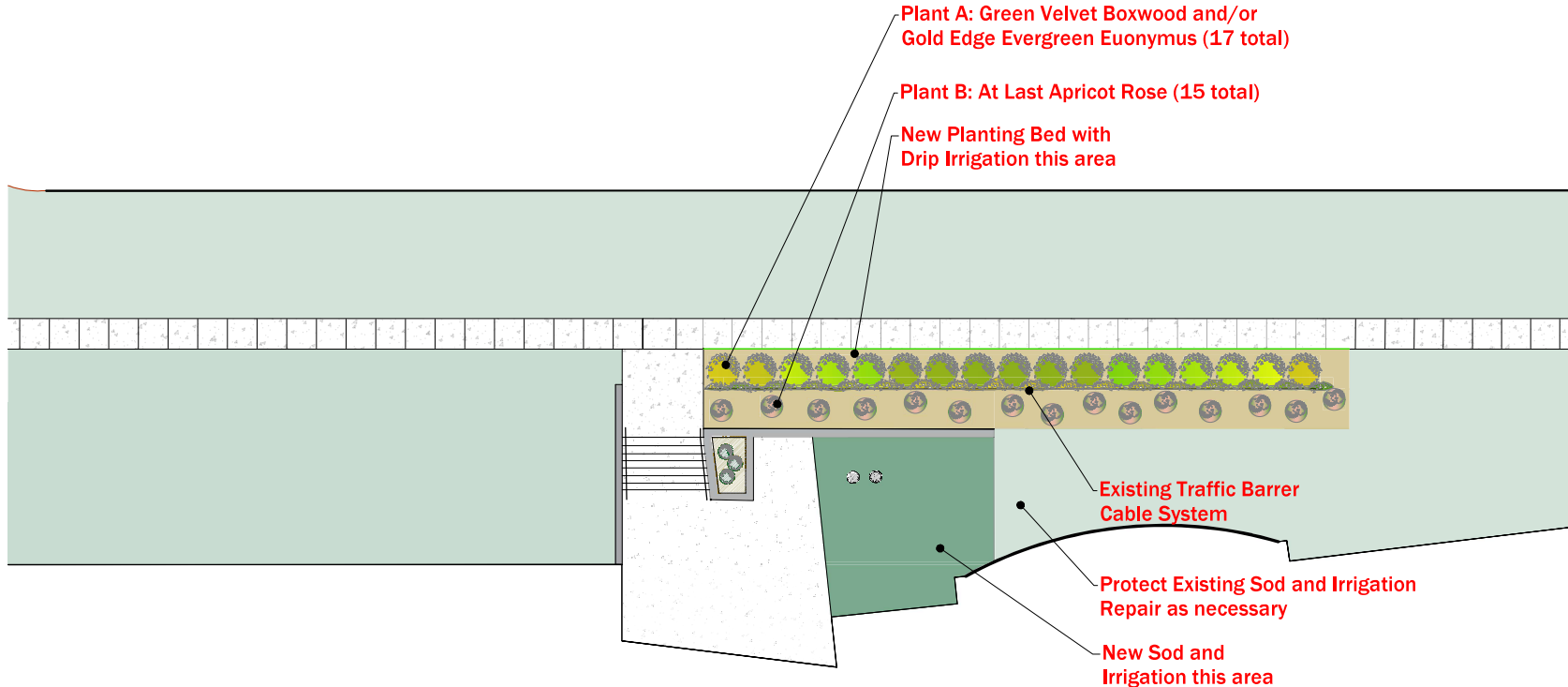


**1** Green Velvet Boxwood  
SCALE: Not to Scale



**1** At Last Apricot Rose  
SCALE: Not to Scale

See Spec 4 for Information on Plant Species



**1** Site Planting & Irrigation Plan  
SCALE: 1/16" = 1' - 0"



1954 A Street, Lincoln, NE 68502  
402-430-5631  
greg.newport@outlook.com

No.	Revision/Issue	Date

Community Action Partnership  
Head Start and Early Head Start  
1843 K Street, Lincoln, NE

**Traffic Barrier  
Planting Plan**



Project	Sheet
Date June 2023	L-1
Scale 1/16" = 1'-0"	





# 32 91 13 - PLANTING BED

## 1. GENERAL

### 1.1 Description:

This specification outlines the requirements for the installation of a planting bed with wood mulch, site preparation, and the planting of shrubs.

### 1.2 Related Documents:

Drawings and specifications, including landscape design plans.

Manufacturer's product data sheets and installation instructions for wood mulch.

Planting schedule and specifications for shrubs.

### 1.3 Quality Assurance:

All work shall conform to industry standards, local regulations, and manufacturer's recommendations.

Shrubs shall be healthy, disease-free, and of the specified container size.

## 2. SITE PREPARATION

### 2.1 Clearing and Grubbing:

Remove all vegetation, debris, rocks, and other obstructions within the designated planting bed area.

Ensure the bed area is free from any existing plants, roots, or unwanted materials.

### 2.2 Grading:

Properly grade the planting bed area to achieve the desired slope, drainage, and contours as specified in the landscape design plans.

### 2.3 Weed Control:

Apply a pre-emergent herbicide to the planting bed area to control weed growth before the installation of wood mulch.

Ensure that the herbicide used is suitable for the intended application and is applied following manufacturer's guidelines.

## 3. PLANTING BED INSTALLATION

### 3.1 Edging:

Install a suitable edging material, such as metal or plastic, around the perimeter of the planting bed to provide a defined border and prevent the spreading of mulch.

### 3.2 Soil Preparation:

Loosen and amend the soil in the planting bed area to improve its fertility, drainage, and overall health as specified in the landscape design plans.

Incorporate organic matter or soil amendments if required to enhance the soil quality.

### 3.3 Shrub Planting:

Dig holes in the planting bed area according to the specified spacing and depth indicated in the landscape design plans.

Remove the shrubs from their 1-gallon containers and gently loosen the root ball before planting.

Place the shrubs in the holes, ensuring that the top of the root ball is level with or slightly above the surrounding soil.

Backfill the holes with soil, firming it gently around the roots to eliminate air pockets.

Water the newly planted shrubs thoroughly to settle the soil.

### 3.4 Mulch Installation:

Spread a layer of wood mulch with a depth of [specify depth] evenly over the prepared planting bed area, taking care not to mound the mulch against the shrub stems.

Ensure that the mulch is applied to a consistent thickness to provide adequate moisture retention, weed suppression, and insulation.

### 3.5 Mulch Maintenance:

Regularly monitor the mulch layer and replenish it as necessary to maintain the desired thickness and appearance.

Remove any weeds, debris, or unwanted materials that accumulate on the mulch surface.

## 4. CLEANUP

### 4.1 Site Cleanup:

Remove and dispose of all excess soil, debris, and materials generated during the installation process.

Leave the site clean and free from any construction-related waste.

## 5. WARRANTIES

### 5.1 Manufacturer's Warranty:

The wood mulch manufacturer shall provide a warranty for the product used, covering defects in materials and workmanship.

### 5.2 Plant Warranty:

The shrub supplier shall provide a warranty for the shrubs planted, covering their health and viability for one year from date of installation.

End of Section



Gregory Newport Architect, LLC

1954 A Street, Lincoln, NE 68502  
402-430-5631  
greg.newport@outlook.com

No.	Revision/Issue	Date

**Community Action Partnership  
Head Start and Early Head Start  
1843 K Street, Lincoln, NE**

**Traffic Barrier  
Planting Plan**



<b>Project</b>	<b>Sheet</b>
<b>June 2023</b>	<b>Spec 2</b>
<b>No Scale</b>	



## 32 92 23 - SODDING

### 1. General

#### 1.1 Description:

This specification outlines the requirements for lawn sodding, including the preparation of the area, installation of sod, and establishment of a healthy lawn.

#### 2. Materials:

High-quality turfgrass sod appropriate for the local climate and soil conditions.

Topsoil suitable for promoting healthy root growth.

Fertilizers, as recommended by a qualified agronomist or landscape professional.  
Mulch or straw for protecting newly installed sod.

#### 3. Site Preparation:

Clear the area of existing vegetation, debris, and rocks.

Grade the site to ensure proper drainage.

Remove any weeds or unwanted plant materials.

Test the soil pH and make necessary adjustments to achieve optimal conditions for the selected turfgrass.

#### 4. Sod Installation:

Install the sod as soon as possible after delivery.

Lay the sod in a brick-like pattern to minimize visible seams.

Ensure tight and uniform joints between each sod piece.

Avoid stretching or overlapping sod during installation.

Use a roller to press the sod firmly into the soil to ensure good root-to-soil contact.  
Trim excess sod to fit irregular edges and boundaries.

### 4. Watering and Maintenance:

Water the newly installed sod immediately after installation to ensure proper establishment.

Provide regular irrigation to keep the sod moist for the first two weeks.

Adjust the watering schedule based on weather conditions and the sod's moisture requirements. Apply fertilizers and soil amendments according to the recommendations of a qualified professional.

Monitor for pests, diseases, and weed growth and take appropriate measures for control.

Mow the lawn at the appropriate height, ensuring not to remove more than one-third of the grass blade in a single mowing.

### 5. Environmental Considerations:

Comply with local regulations regarding water usage and potential runoff.

Properly dispose of any excess soil, sod, or other materials generated during the project.

Minimize the use of synthetic fertilizers and pesticides, opting for organic alternatives whenever possible.

### 6. Quality Assurance:

Inspect the sod upon delivery for freshness, uniformity, and absence of weeds or diseases.

Verify the proper installation of the sod, including tight joints and good root-to-soil contact.

Conduct periodic inspections during the establishment phase to identify and address any issues promptly.

End of Section

## 32 93 22 Shrubs

### 1. General

#### 1.01 Scope of Work

This section includes the requirements for the supply and installation of shrubs for landscaping purposes.

#### 1.02 Related Sections

Related sections:

32 84 13 - Drip Irrigation  
32 91 13 - Planting Bed

#### 1.03 References

American Nursery and Landscape Association (ANLA) International Society of Arboriculture (ISA) local and regional landscaping codes and regulations

#### 1.04 Submittals

Submit the following documents for approval prior to commencing work:

Planting plan indicating shrub types, quantities, and locations

Product data sheets for specified shrub varieties  
Maintenance and care instructions

#### 1.05 Quality Assurance

Comply with local and regional landscaping codes and regulations.

Provide shrubs that are healthy, free from pests, and in good condition at the time of installation.

Obtain shrubs from reputable nurseries with demonstrated expertise in shrub cultivation.

### 2. Products

#### 2.01 Shrub Selection

Select shrub species and varieties suitable for the local climate, soil conditions, and intended landscaping design.

Provide a diverse range of shrubs that offer visual interest, varying heights, and seasonal color.

#### 2.02 Planting Materials

Provide shrubs with well-developed root systems, appropriate for the size of the plants.

Use organic soil amendments, compost, or other suitable materials to promote healthy plant growth.

#### 2.03 Planting Method

Dig holes of sufficient depth and width to accommodate the root systems.

Backfill with amended soil to provide a stable and supportive growing environment.  
Compact the soil gently around the shrubs to eliminate air pockets.

### 3. Execution

#### 3.01 Preparation

Clear the planting areas of any existing vegetation, debris, and rocks.

Grade the soil as necessary to ensure proper drainage.

Amend the soil with organic matter to improve fertility and structure, if needed.



Gregory Newport Architect, LLC

1954 A Street, Lincoln, NE 68502  
402-430-5631  
greg.newport@outlook.com

No.	Revision/Issue	Date

Community Action Partnership  
Head Start and Early Head Start  
1843 K Street, Lincoln, NE

Traffic Barrier  
Planting Plan



<b>Project</b>	<b>Sheet</b>
<b>Date</b> June 2023	<b>Spec 3</b>
<b>Scale</b> No Scale	

**3.02 Installation**

Install shrubs in accordance with the approved planting plan and in proper sequence with other work.

Ensure proper spacing between shrubs to allow for their mature growth and prevent overcrowding.

Orient shrubs in a visually appealing manner, considering their shape, form, and characteristics.

**3.03 Backfilling and Mulching**

Backfill each planting hole with amended soil, ensuring it is firmly packed around the shrub's root ball.

Apply a layer of organic mulch around the base of each shrub to conserve moisture, suppress weeds, and moderate soil temperature.

**3.04 Watering**

Water shrubs immediately after installation to settle the soil.

Provide adequate irrigation during the establishment period, following industry best practices and local water regulations.

Monitor soil moisture levels and adjust watering frequency as necessary.

**3.05 Clean-up**

Remove all debris, excess soil, and construction materials from the site upon completion.

Dispose of waste in accordance with local waste management regulations.

**4. Maintenance**

**4.01 Maintenance Responsibilities**

The contractor is responsible for shrub maintenance until final acceptance by the owner or designated authority.

Maintain shrubs in healthy condition, free from pests, diseases, and weeds.

**4.02 Pruning and Trimming**

Conduct regular pruning and trimming of shrubs to maintain their desired shape, size, and aesthetics.

Follow accepted industry practices to avoid causing harm to the shrubs.

**4.03 Fertilization and Nutrient Management**

Provide appropriate fertilization based on the specific needs of the shrub species and soil conditions.

Monitor nutrient levels and apply fertilizers in accordance with local regulations and best practices.

**4.04 Pest and Disease Control**

Implement integrated pest management (IPM) techniques to control pests and diseases effectively.

Utilize environmentally friendly methods and products whenever possible, in compliance with local regulations.

**4.05 Reporting**

Provide regular reports on maintenance activities, including tasks performed, materials used, and any issues encountered.

**5. Warranty**

**5.01 Warranty Period**

Provide a warranty for shrubs and their installation for a period of one year from the date of substantial completion.

The warranty should cover defects in materials, workmanship, and the overall health and viability of the shrubs.

**5.02 Warranty Remedies**

The contractor shall promptly replace any shrubs that fail to thrive or meet the specified requirements during the warranty period.

The replacement shall be of similar quality and size as the original installation.

End of Section

 Gregory Newport Architect, LLC	
1954 A Street, Lincoln, NE 68502 402-430-5631 greg.newport@outlook.com	
<p><b>Community Action Partnership Head Start and Early Head Start 1843 K Street, Lincoln, NE</b></p>	
<p><b>Traffic Barrier Planting Plan</b></p>	
<p>Project _____ Sheet _____</p>	
<p>Date <b>June 2023</b> Spec <b>4</b></p>	
<p>Scale <b>No Scale</b></p>	

## NEBRASKA CAPITOL ENVIRONS COMMISSION STAFF REPORT

APPLICATION NUMBER	Urban Design Record #23091
APPLICATION TYPE	Certificate of Appropriateness
ADDRESS/LOCATION	Lincoln Mall right-of-way in front of 1023 Lincoln Mall
HEARING DATE	July 28, 2023
ADDITIONAL MEETINGS	Discussion at June NCEC meeting (06/23/23)
APPLICANT	Stephanie Rouse, Transportation Planner, 402-441-6373, <a href="mailto:srouse@lincoln.ne.gov">srouse@lincoln.ne.gov</a>
STAFF CONTACT	Collin Christopher, 402-441-6370, <a href="mailto:cchristopher@lincoln.ne.gov">cchristopher@lincoln.ne.gov</a>

**RECOMMENDATION: N/A**

### Summary of Request

Early in 2022 the Complete Streets Committee began researching a bike rack request program to launch in Lincoln. According to the Lincoln Bike Plan adopted in 2019, “Providing secure and convenient locations for people to leave their bicycles while working, running errands, or socializing is a crucial supporting component to an effective bicycle network. Short-term bike parking should be placed in a visible area close to the destination it is serving, ideally within 50 feet of the entrance, and easy to use. Many rack styles exist, with an inverted “U” design most common; the City should stick to one style as much as possible for consistency.”

Over the course of a year, the committee finalized the program as a more targeted effort to install 33 racks throughout the city. Many of the locations are in business districts such as College View or Havelock, however a few locations were selected based on demonstrated need by individual organizations. One of those requests came from Lincoln Literacy who has a demonstrated need for racks at their location to serve visitors. At their previous location on 10<sup>th</sup> Street they had numerous clients arriving by bicycle and had originally requested racks at that location.

While the two racks proposed would be in front of the new Lincoln Literacy building at 1023 Lincoln Mall, they would be in the public right-of-way and accessible to anyone to use, not just Lincoln Literacy visitors. Though the Committee looked at locations behind the building, ultimately there wasn’t enough space to locate them adjacent to the parking and still provide pedestrian access along the sidewalk. Removing one of the limited number of parking spaces behind the building was also considered, but was ultimately determined to not be a preferable option. Finally, per the direction of the Commission, using a parking stall on 11<sup>th</sup> Street was explored, but it was determined that the location would not be easy to find for visitors along Lincoln Mall and that there are already existing racks nearby on H Street. Removing a parking stall on 11<sup>th</sup> is also challenging due to the parking demand along the corridor.

The racks are silver, U-shaped bike racks (four bike capacity) and would be bolted into the existing concrete. The U-shaped design is more compatible with modern standards for locking bikes than the post and hoop style installed at 14<sup>th</sup> Street and Lincoln Mall (see Attachment C). Per recommended installation guidance, the racks would be about 2-3 feet from the sidewalk to allow for overhang of the tires and 72 inches apart from one another, installed perpendicular to the sidewalk to allow ample space for pedestrians to walk to the building entrance.

<https://linclanc.sharepoint.com/sites/PlanningDept-Boards/Shared Documents/Boards/NCEC/REPORTS/2023/07-July/1023 Lincoln Mall staff report.docx>

## Compatibility with the Design Standards

The design standards are silent on the installation of bike racks. However, as provided below, the standards do make references to activating the Malls through seating and other landscape elements.

### Chapter 3.85, Design Standards 18: General Landscape of All Malls

The landscape of the Capitol Malls should enhance the Capitol setting and vistas:

- by providing canopy and definition at the mall edge, but leaving the centers of the malls more open, allowing on-axis views of the Capitol,
- by creating a sense of organization and unity through form, color, texture, and spacing that may be lacking in the adjacent architecture,
- by establishing a rhythm of visual and physical movement leading ultimately to the Capitol,
- by providing seasonal change and interest.

There is a delicate balance between a landscape that enhances and one that overwhelms—color and form changes should be in large sweeps and masses to avoid creating small, distracting focal points. The landscape should be bolder and more colorful closest to the Capitol.

Facing Capitol Square and the Malls, front yards shall not contain mechanical equipment, above-ground utilities, docks, or unscreened ramps. Architectural or planted screenings may be offered, with maintenance requirements.

Active use of the Malls should be encouraged by the selection and placement of landscape elements such as seating.

**Compatibility per Staff Analysis:** The standards do not make specific references to bike racks on the malls. In staff's opinion, this silence should not necessarily be interpreted to say that bike racks should or should not be allowed on Lincoln Mall. Instead, it may be more beneficial to consider precedent as it relates to bike racks on the malls.

## Recommendations

This proposal, while relatively minor in its immediate impact on Lincoln Mall, is a difficult one to weigh because of the impact it could have on future decisions. The design standards do not directly speak to the use of bike racks on the malls, nor do they speak to the use of some other common site furnishings and amenities like trash receptacles or bus stop shelters. Yet, each of those elements (including bike racks) exist on Lincoln Mall today. In 2021, NCEC approved the addition of bike lanes to Lincoln Mall, though no additional racks were added to the public right-of-way at that time. Looking beyond Lincoln Mall, Centennial Mall also has many examples of bike racks being integrated into the streetscape.

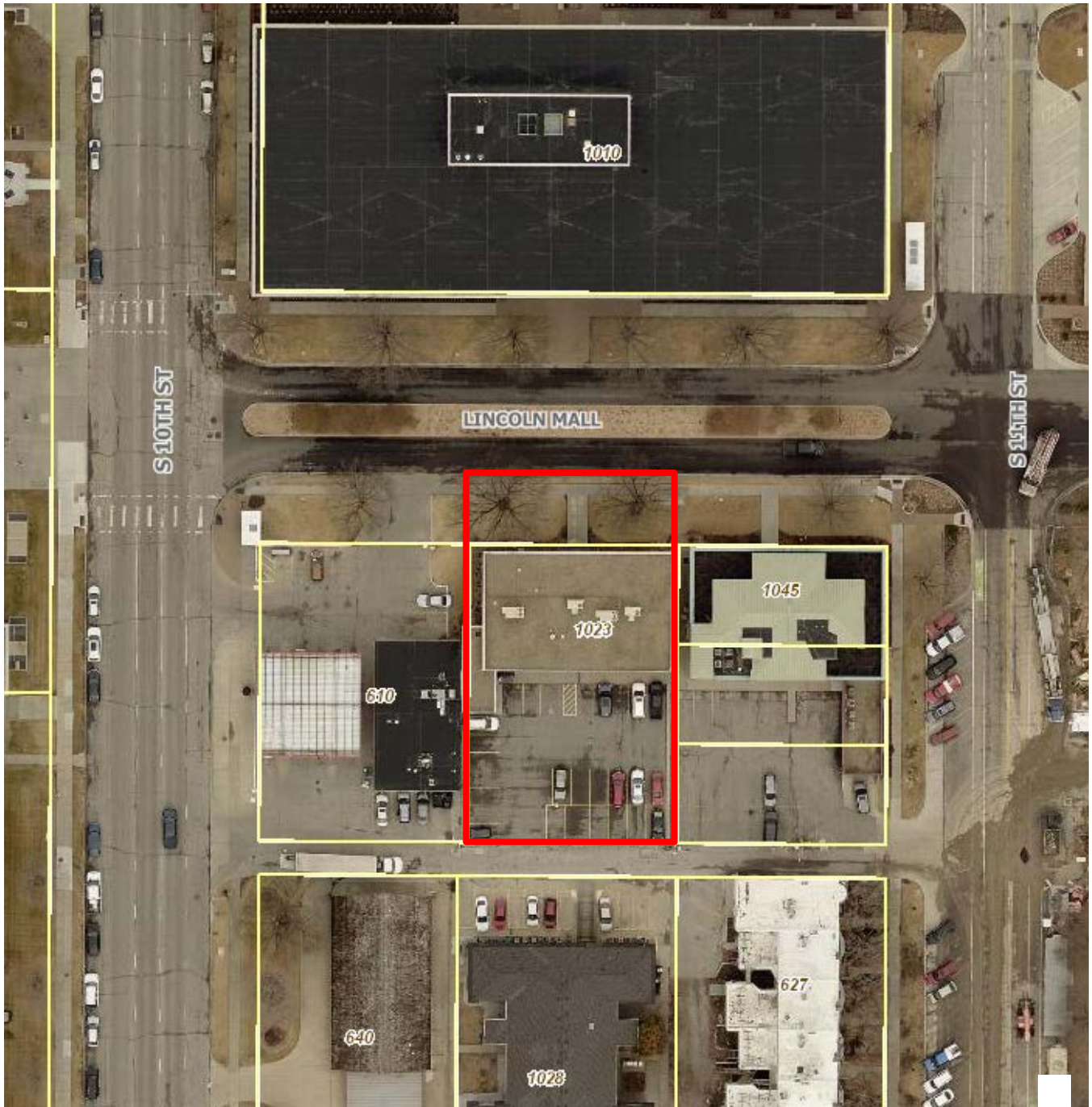
On the other hand, there is a clear precedent that has been set with the NEBCO redevelopment projects along Lincoln Mall and the related public streetscape improvements to locate the bulk of the bike parking behind the buildings fronting the mall, where the vehicular parking exists. Though there is nothing that binds this decision to previous improvements, they can certainly be viewed as a guide for consideration.

In the end, staff believes that this decision comes down to how the bike racks are perceived to fit within the urban design context of Lincoln Mall. Are they more of a site furnishing that helps to activate the mall or are they parking that should be relegated to the rear? Sometimes there appears to be a tendency to choose preservation of pristine views of the Capitol over people-focused urban design solutions. Regardless of the decision for this particular proposal, the Commission is encouraged to weigh that balance and offer staff guidance on how to appropriately update the design standards in the future to convey such considerations.

Staff is not offering a recommended finding or action here because the design standards do not provide adequate guidance to make such recommendations. Instead, staff is asking the Commission to consider the request and offer both a decision on this particular proposal as well as guidance for addressing future proposals within the broader context summarized in this staff report.



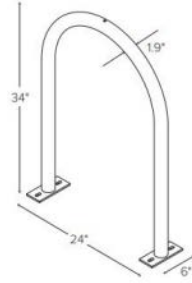
ATTACHMENT A – Location Map



UDR23091 – Bike racks on Lincoln Mall (in front of 1023 Lincoln Mall)



## ATTACHMENT B – Proposed Bike Rack Details and Imagery



*Bike rack dimensions*



*Proposed location of two racks in red*



*View of two racks placed in front of building*

## ATTACHMENT C – Existing Bike Racks



*Post and bollard racks installed at the northwest corner of 14<sup>th</sup> Street and Lincoln Mall*